

Shipping—Steamers.

HONGKONG, CANTON, MACAO AND WEST RIVER STEAMERS.

JOINT SERVICE OF THE HONGKONG, CANTON AND MACAO STEAMBOAT CO., LTD., AND THE CHINA NAVIGATION COMPANY, LTD.

HONGKONG-CANTON LINE.

S.S. "HONAM" 2,363 tons..... Captain H. D. Jones.
 "POWAN" 2,338 "..... " G. F. Morrison, R.M.R.
 "PATSHAN" 2,350 "..... " R. D. Thomas.
 "KINSHAN" 2,073 "..... " C. V. Lloyd.
 "KINSHAN" 1,995 "..... " J. J. Lottus.
 Departures from HONGKONG to CANTON daily at 8.30 A.M. (Sunday excepted), 9 P.M. and 10.30 P.M. (Sunday excepted).
 Departures from CANTON to HONGKONG daily at 8.30 A.M., 3 P.M. and 6 P.M. (Sunday excepted).
 These Steamers, carrying His Majesty's Mails, are the largest and fastest on the River. Special attention is drawn to their Superior Saloon and Cabin accommodation.

SERVICE OF THE HONGKONG, CANTON AND MACAO STEAMBOAT CO., LTD. HONGKONG-MACAO LINE.

S.S. "HEUNGSHAN" 1,998 tons..... Captain W. E. Clarke.
 Departures from Hongkong to Macao on week days about 2 P.M. (See special Summer Time-table). Departures on Sundays at Noon.
 Departures from Macao to Hongkong daily at 8 A.M.

CANTON-MACAO LINE.

S.S. "LUNGSHAN" 2,119 tons..... Captain T. Hamlin.
 This steamer leaves Canton for Macao every Tuesday, Thursday and Saturday at about 8.30 A.M.; and leaves Macao for Canton every Monday, Wednesday and Friday at about 8 A.M.

JOINT SERVICE OF THE H.K. C. AND MACAO STEAMBOAT CO., LTD. THE CHINA NAVIGATION COMPANY, LTD., AND THE INDO-CHINA STEAM NAVIGATION COMPANY, LTD.

CANTON-WUCHOW LINE.

S.S. "SAINAM" 388 tons..... Captain W. A. Valentine.
 "NANNING" 369 "..... " G. Butchart.
 One of the above steamers leaves Canton for Wuchow every Monday, Wednesday and Friday at about 8.30 A.M. calling at Yunkai, Mahning, Kumchuk, Kau-Kong, Samshui, Howlik, Shui-Hing, Luk-Lo, Lo-Ting-Hau, Tak-Hing, Doshing and Fong-Chuen. Departures from Wuchow for Canton calling at the above ports every Monday, Wednesday and Friday at about 8.30 A.M.
 FARES:—Canton to Wuchow.....Single \$15.00. Return \$25.00.
 Canton to Tak Hing.....Single \$12.50. Return \$21.00.
 Canton to Samshui.....Single \$7.50.

HONGKONG-WUCHOW LINE.

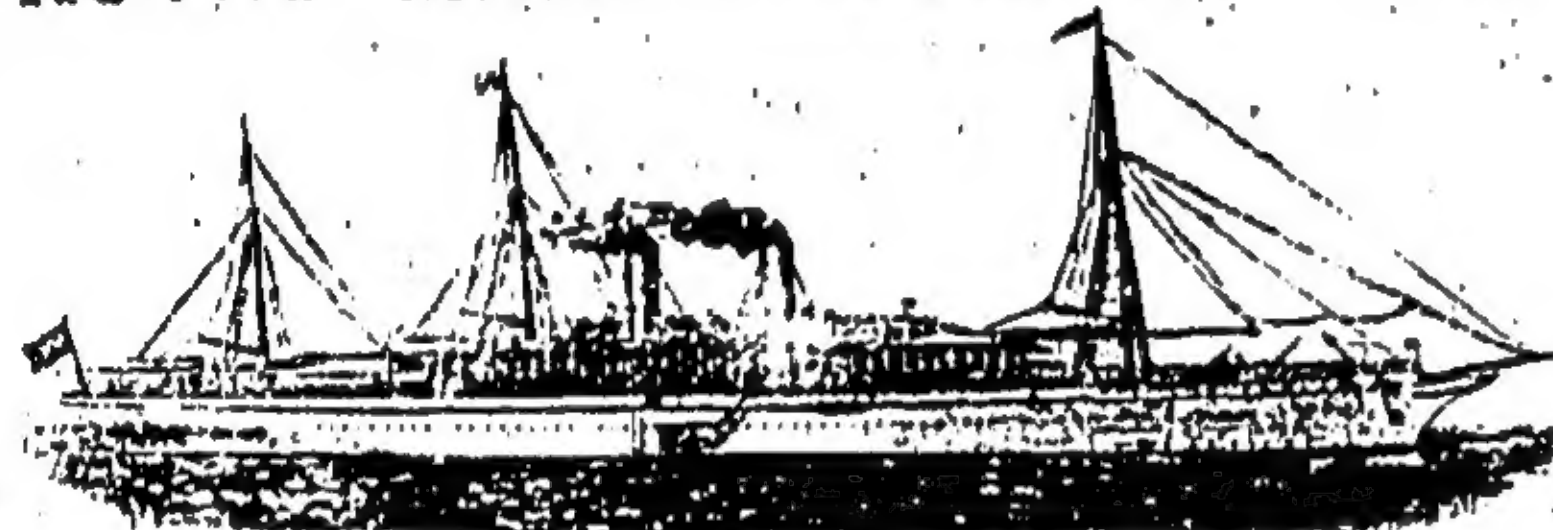
S.S. "LINTAN" Capt. B. Branch. S.S. "SANUI" Capt. J. Willox.
 Departures from Hongkong to Wuchow about three times every week, calling at Kumchuk, Samshui, Shui-Hing and Tak-Hing. Departures from Wuchow about three times every week calling at the same ports.
 FARES:—Hongkong to Wuchow Single \$17.50. Return \$30.00. Round trip tickets to Wuchow returning via Canton or vice versa \$36.00, available for one month. Round trips to and from Wuchow take from 5 to 7 days.

HONGKONG-KONGMOON LINE.

S.S. "TAK HING" Capt. R. Biers. S.S. "HONGKONG" Capt. Maxfield.
 Departures from Hongkong daily (Saturday excepted) at 7 P.M., calling at Kumchuk and Kongmoon. Returning daily (Monday excepted).
 FARES:—Hongkong to Kong Moon.....Single \$6.00.
 Hongkong to Kumchuk.....Single \$7.00.
 The above vessels have superior Saloon and Cabin accommodation and are lighted throughout by electricity. Meals charged extra.
 Further particulars may be obtained at the Office of the—
 HONGKONG, CANTON & MACAO STEAMBOAT CO., LD.,
 18, Bank Buildings, Queen's Road Central, opposite the Hongkong Hotel
 Or of BUTTERFIELD & SWIRE,
 Agents, CHINA NAVIGATION CO., LTD.

Hongkong, 17th June, 1905.

CANADIAN PACIFIC RAILWAY COY.'S ROYAL MAIL STEAMSHIP LINE.



THE FAST ROUTE BETWEEN CHINA, JAPAN AND EUROPE, VIA CANADA AND THE UNITED STATES.
 (CALLING AT SHANGHAI, NAGASAKI, KOBE, YOKOHAMA & VICTORIA, B.C.)
 SAVING 1 TO 7 DAYS ACROSS THE PACIFIC.
 PROPOSED SAILINGS FROM HONGKONG.—(SUBJECT TO ALTERATION).

Steamers.	Tons.	Commanders.	Sailing Dates.
R.M.S. "EMPRESS OF INDIA".....	6,000	E. Beetham, R.M.R.	WEDNESDAY, 21st June.
"TARTAR".....	4,425	W. Davison, R.M.R.	WEDNESDAY, 5th July.
"EMPRESS OF JAPAN".....	6,000	H. Pybus, R.M.R.	WEDNESDAY, 12th July.
"EMPRESS OF CHINA".....	6,000	R. Archibald, R.M.R.	WEDNESDAY, 2nd Aug.
"ATHENIAN".....	3,440	S. Robinson, R.M.R.	WEDNESDAY, 9th Aug.

Hongkong to London, 1st Class.....via St. Lawrence £60. via New York £65.
 Hongkong to London, Intermediate or 2nd Class....." £40. " £45.
 Steamers, and 1st Class Rail....." £40. " £45.

THE magnificent Twin-screw "EMPRESS" Steamships pass through the famous INLAND SEA OF JAPAN, and usually make the voyage YOKOHAMA TO VANCOUVER (B.C.) in 12 DAYS, and make connection with the PALATIAL OVERLAND TRAINS FROM THE PACIFIC TO THE ATLANTIC WITHOUT CHANGE.
 R.M.S. "TARTAR" and "ATHENIAN" carry "Intermediate" Passengers only at intermediate rates, affording superior accommodation for that class.
 Passengers Booked through to all principal points and AROUND THE WORLD.
 SPECIAL RATES (First class only) granted to Missionaries, Members of the Naval, Military, Diplomatic and Civil Services, and to European Officials in the Service of Chinese and Japanese Governments.
 For further information, Maps, Guides, Hand Books, Rates of Freight and Passage, apply to
 Hongkong, 14th June, 1905. D. E. BROWN, General Agent, 9, Fiddler's Street.

HAMBURG-AMERIKA LINIE.

OBTASTISCHER DIENST.

(Taking Cargo at through Rates to ANTWERP, AMSTERDAM, ROTTERDAM, COPENHAGEN, LISBON, Oporto, LONDON, LIVERPOOL, GLASGOW, TRIESTE, GENOA, PORTS IN THE LEVANT; BLACK SEA AND BALTIC PORTS; NORTH AND SOUTH AMERICAN PORTS).

PROPOSED SAILINGS FROM HONGKONG SUBJECT TO ALTERATION.

STEAMERS.	DESTINATIONS.	SAILING DATES.
C. FERD. LAEISZ von Hoff	H'VRE, DUNKIRK, BREMEN & HAMBURG. (Calling at S'PORE, PENANG & COLOMBO.)	22nd June. } Freight.
BRISGAVIA	HAVRE, ANTWERP & HAMBURG. (Calling at S'PORE, PENANG & COLOMBO.)	30th June. } Freight.
SITHONIA	HAVRE, BREMEN & HAMBURG. (Calling at S'PORE, PENANG & COLOMBO.)	12th July. } Freight.
ACILIA	HAVRE & HAMBURG. (Calling at S'PORE, PENANG & COLOMBO.)	26th July. } Freight.
ALESIA	HAVRE & HAMBURG. (Calling at S'PORE, PENANG & COLOMBO.)	16th August. } Freight.

For further Particulars, apply to

HAMBURG-AMERIKA LINIE, HONGKONG OFFICE, No. 1, Queen's Buildings.

Hongkong, 15th June, 1905.

D. NOMA, TATTOOER,

60, QUEEN'S ROAD CENTRAL.

THE Public are informed that my Parlours are open from 9 A.M. all day. My 33 years' experience in TATTOOING is a guarantee of good work and prompt execution. My Colours are absolutely fast and perfectly harmless, and produce a charming effect not attained by any other, as their composition is only known to me. H. R. M. The Duke of York, and H. I. H. The Emperor of Russia, both honoured me with their patronage besides many others of High Rank. Prices Moderate and satisfaction guaranteed as attested by 3,700 Recommendations which I have received from all sources.
 Hongkong, 16th November, 1904.

Mails.

IMPERIAL GERMAN MAIL LINES.

NORDDEUTSCHER LLOYD, BREMEN.

EUROPEAN LINE.

STEAM FOR SINGAPORE, PENANG, COLOMBO, ADEN, SUER, PORT SAID, NAPLES, GENOA, ANTWERP, BREMEN/HAMBURG.
 PORTS IN THE LEVANT, BLACK SEA AND BALTIC PORTS.
 ALSO LONDON, NEW YORK, BOSTON, BALTIMORE, NEW ORLEANS, GALVESTON, AND SOUTH AMERICAN PORTS.
 Steamers will call at GIBRALTAR and SOUTHAMPTON to land Passengers and Luggage.

N.B.—Cargo can be taken on through Bills of Lading for the Principal Places in Russia.

PROPOSED SAILINGS FROM HONGKONG.

(SUBJECT TO ALTERATION.)

STEAMERS.	SAILING DATES.
ZIETEN	WEDNESDAY, 21st June.
DARMSTADT	WEDNESDAY, 5th July.
SACHSEN	WEDNESDAY, 19th July.
SCHARNHORST	WEDNESDAY, 2nd August.
PRINZ HEINRICH	WEDNESDAY, 16th August.
PRINZ EITEL FRIEDRICH	WEDNESDAY, 30th August.
BRUSSEN	WEDNESDAY, 13th September.
BOYEN	WEDNESDAY, 27th September.
GNEISENAU	WEDNESDAY, 11th October.
PRINZESS ALICE	WEDNESDAY, 25th October.
SACHSEN	WEDNESDAY, 8th November.
PRINZ REGENT LUITPOLD	WEDNESDAY, 22nd November.
PRINZ HEINRICH	WEDNESDAY, 6th December.

ON WEDNESDAY, the 21st day of June, 1905, at Noon, the Steamship "ZIETEN" of the NORDDEUTSCHER LLOYD, Captain F. von Blücher, with MAILED PASSENGERS, SPECIE and CARGO, will leave this Port as above, calling at NAPLES and GENOA. Shipping Orders will be granted till Noon, on MONDAY the 19th June, Cargo and Specie will be received on Board until 5 P.M., on TUESDAY, the 20th June, and Parcels will be received at the Agency's Office until Noon, on TUESDAY, the 20th June. Contents of Packages are required. No Parcel Receipts will be signed for less than \$2.50 and Parcels should not exceed 20 Cubic Feet in Measurement.

The Steamer has splendid Accommodation and carries a Doctor and Stewardsess. Linen can be washed on board.

JAPAN-CHINA-AUSTRALIA LINE, VIA NEW GUINEA.

STEAM FOR FRIEDRICH-WILHELMSHAFEN, HERBERTSHOEHE, MATUPI, BRISBANE, SYDNEY AND MELBOURNE.

PROPOSED SAILINGS FROM HONGKONG.

(Subject to alteration.)

STEAMERS.	Tons.	SAILING DATES.
WILLEHAD	4,761	TUESDAY, 27th June.
PRINZ WALDEMAR	3,327	TUESDAY, 15th July.
PRINZ SIGISMUND	3,302	TUESDAY, 22nd August.

ON TUESDAY, the 27th June, 1905, at Noon, the Steamship, WILLEHAD, Captain Ph. Oberauer, with Mails, Passengers and Cargo, will leave this port as above. The steamer has splendid accommodation and carries a Doctor and a Stewardsess. Linen can be washed on board.

SAILINGS OUTWARDS.

EUROPEAN & AUSTRALIAN SERVICE.

FOR	STEAMERS	ABOUT
KOBE & YOKOHAMA	PRINZ WALDEMAR	TUESDAY, 20th June.
SHANGHAI, NAGASAKI, KOBE & YOKOHAMA	SACHSEN	WEDNESDAY, 21st June.
SHANGHAI, NAGASAKI, KOBE & YOKOHAMA	SCHARNHORST	WEDNESDAY, 5th July.

NORDDEUTSCHER LLOYD.

For further Particulars, apply to

MELCHERS & CO., AGENTS.

Hongkong, 7th June, 1905.

Intimations.

THE HONGKONG STUDIO,

HIGHER CLASS PHOTOGRAPHER, 41 & 43, QUEEN'S ROAD CENTRAL, TOP FLOOR.

PORTRAITS, GROUPS AND ENLARGING AND COPYING in all Sizes.

LARGE SELECTION OF VIEWS ALWAYS ON HAND.

PRICE VERY MODERATE.

Hongkong, 15th September, 1905.

FURNITURE WAREHOUSE.

LI KWONG LOONG,

CABINET-MAKER AND ART DECORATOR, from Shanghai, has opened a FURNITURE STORE at

No. 45, DES VOUX ROAD CENTRAL. The only Shop in Hongkong with this name.

WHERE HIGH-CLASS FURNITURE of every description can be made to order in any design required.

Has been patronised by the Hongkong Club, Hongkong Hotel, Messrs. A. S. Watson & Co., Ltd., Joint Telegraphs Co., and other leading Establishments in the Colony, to whom reference may be made as to the Superior Workmanship and Materials of the Furniture, &c., supplied.

Messrs. A. S. Watson & Co., Ltd. write as follows:—
 "We have pleasure in stating that Mr. LI KWONG LOONG furnished the Annex to our Dispensary and gave us every satisfaction."
 (Sd.) A. S. WATSON & Co., Ltd.
 ORDERS punctually attended to, and CHARGES most moderate.
 AN INSPECTION INVITED.
 Hongkong, 6th December, 1904.

SAVARESSE'S SANDAL CAPSULES
 Cures all kinds of skin diseases, itching, eruptions, etc.
 Sold by Chemists.



THIS DWARF RAZOR has superseded the old fashioned clumsy Razor and by its use shaving becomes a pleasure. It is manufactured in Sheffield, England, from a special amalgam of steel which makes imitation impossible, and in consequence it enjoys the largest sale of any Razor in the World. Thousands of Testimonials testify that the little "MAB" is the finest shaving implement ever produced.

Will be mailed to any address on receipt of the price (\$2), post free.

To be obtained from THE MUTUAL STORES, WATKINS, LIMITED, and all first-class stores in the Colony.

Sole Agents for Far East, HOWARD & Co., 29, Des Voux Road, Central, Hongkong. Agents wanted in every port.

For particulars and terms, apply to—HOWARD & Co. Hongkong, 24th November, 1904.

OCCIDENTAL HOTEL.

EXCELLENT CUISINE.

MODERATE PRICES.

ELECTRIC FANS

TO ORDER IN

EVERY ROOM.

EUROPEAN MANAGEMENT.

ELGIN ROAD, KOWLOON.

Hongkong, 10th May, 1904.



Intimations.

THE YOKOHAMA DOCK CO., LTD.

No. 1 DOCK.

Length inside, 814 ft. Width of entrance, top 95 ft.; bottom 75 ft. Water on blocks, 27.5 ft. Time to pump out, 4 hours.

No. 2 DOCK.

Length inside, 875 ft. Width of entrance, top 60.5 ft.; bottom 45.8 ft. Water on blocks, 26.5 ft. Time to pump out, 2 hours.

THESE DOCKS are conveniently situated in Yokohama harbour and the attention of Captains and Engineers is respectfully called to the advantages offered for Docking and repairing Vessels and Machinery of every description.

The plant and tools are of recent patterns for dealing quickly and cheaply with work and a large stock of material is always at hand, (plates and angles all being tested by Lloyd's surveyors).

Two powerful Twin Screw Towboats are available for taking Vessels in or out of Dock, and for taking Sailing Vessels in or out of the bay. The floating derrick is capable of lifting 35 tons.

Steam Launches of Steel or Wood, Lighters, Steel Buildings and Roofs, Bridge Work, and all kinds of Machinery are made on the premises.

Tenders will be made up when required and the workmanship and material will be guaranteed.

The cost of Docking, and repair work, will be found to compare favourably with that of any port in the world.

Telephone: Nos. 376, 506, or 681.

Telegrams, "Dock, Yokohama," Codes A. B. C. 4th and 5th Editions, Liebers, Scotts, A. I. and Watkins.

Yokohama, May 23rd, 1905.

C. W. MEAD, C.E., President and Shanghai Manager. N. M. HOLMES, C.E., Vice-President and Hongkong Manager. A. F. CARRICK, C.E., General Manager, Manila.

ORIENTAL CONSTRUCTION COMPANY, CONSULTING AND SUPERVISING ENGINEERS AND CONTRACTORS, HONGKONG, SHANGHAI AND MANILA.

Cable Address: WERRICK, HONGKONG.
 Railway Hydraulic Mining and Sanitary Engineering. A Specialty made of Reinforced Concrete and Concrete Piles. Examinations of Surveys, Reports and Estimates. On all Railway or Proposed Construction Works.
 Hongkong, 2nd February, 1905.

"MINIMAX" HAND

FIRE EXTINGUISHER.

MINIMAX SYNDICATE, LIMITED.

LONDON, NEW YORK, BERLIN, HAMBURG, PARIS, VIENNA, MILAN, COPENHAGEN, ANTWERP, &c.

F. BLACKHEAD & CO., LOCAL AGENTS.

The most effective of all Hand Fire Extinguishing Apparatus. NO PUMPS. NO HOSE. AUTOMATIC. Extinguishes Oil, Varnish, Kerosine Oil, Tar, Bedstone. Guaranteed to remain in working order for any length of time. SIMPLEST HANDLING.

Drive in the Knob and the apparatus is in action immediately, sending Spray nearly 40 feet.

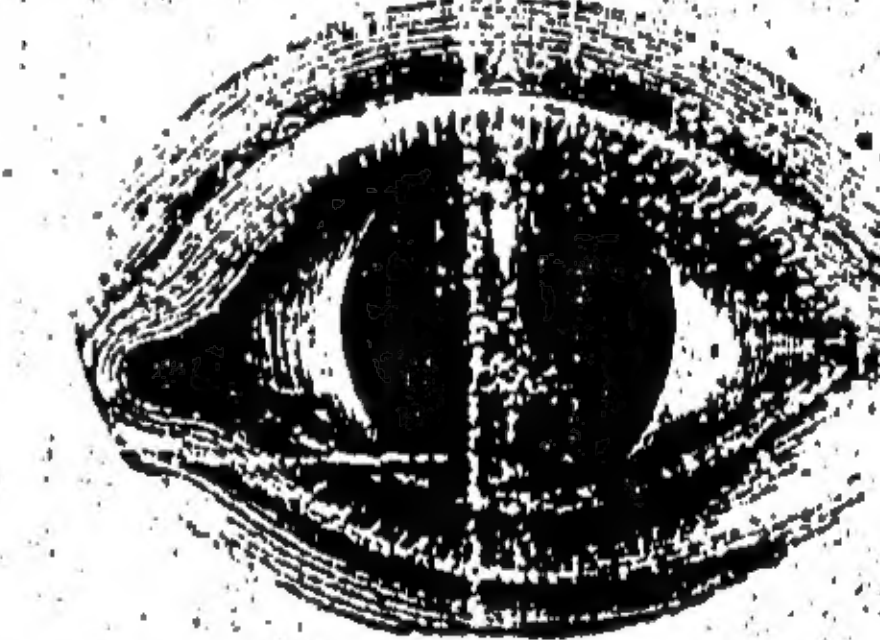
IMPORTANT POINTS FOR CONSIDERATION.

Is Self-acting. Destroys all smoke. Can be used by anyone, even lady or child. Minimum of Price, Weight and Size. Hongkong, 10th May, 1905.

"MINIMAX"

Always ready for immediate use. Requires only one hand to hold. Weight only 18 lbs. when full. Maximum of simplicity and effect.

EYES



RIGHT!

N. LAZARUS, OPHTHALMIC OPTICIAN, 10, D'AGUIAR STREET, HONGKONG, (One Minute's Walk from the Post Office).

WILL test your eyes free of charge, and if they are wrong will put them right.

Lenses Ground. All kinds of Repairs. Spectacles for all requirements. Ask, or write, for Illustrated Booklet on "Defective Sight"—free. LONDON, CALCUTTA, SHANGHAI, 21, John Street, Bedford Row, W.C., 59, Bentinck Street, 566, Nanking Road. Hongkong, 24th March, 1904.

Hotels.

CONNAUGHT HOTEL, HONGKONG.

A FIRST-CLASS FAMILY AND COMMERCIAL HOTEL, situated near the BANK OF CHINA, PRINCIPAL OFFICES and in the MAIN STREET.

Large and lofty Rooms, Elegantly Furnished. Flush Water Lavatories. Hydraulic Elevator. Excellent Cuisine and Wines. Hot and Cold Water Baths and Shower Baths. Under European Management. Launch Service for Guests.
 Hongkong, 16th June, 1905.

FOR HOTEL COMFORT AND THE BEST BILLIARDS

GO TO THE

KOWLOON HOTEL, KOWLOON. J. W. OSBORNE, Proprietor and Manager.

Plunkett's Gap, the Peak, near the TRAM TERMINUS, Tel. 56. For Terms, &c., apply to the MANAGER.
 Hongkong, 2nd July, 1900.

TAIN TING.

LATEST METHODS OF DENTISTRY.

STUDIO AT NO. 14, D'AGUIAR STREET.

REASONABLE FEES.

Consultation Free.

Hongkong, 10th July, 1904.

THE AMERICAN SYSTEM OF DENTISTRY.

M. H. CHAU, D.D.S.

17, DES VOUX ROAD CENTRAL, HONGKONG, From the University of Pennsylvania, U.S.A.

Hongkong, 4th June, 1904.

Hongkong, 17th June, 1905.

—2— KRUPPS employs 30,000 men at Essen, many. War is a good thing for Krupps.

dedicated to seek parasites new. Shanghai seems, has no idea of the capabilities of Grill-room under an experienced and popular manager, but that reproach will be removed very shortly from the' Northern Settlement. Mr. Daly leaves by the *Minnesota* in a day or two for Shanghai, where he will establish a up-to-date grill-room, where the "bucko" Shanghai may be expected to congregate. It is probable that the new establishment, open in the Nanjing Road; but no matter where it is situated, if anything like the conditions are in force there—and it is said that things will be done in Shanghai on a much bigger scale than was attempted in Hongkong—the success of the venture is assured. Many who knew Dick Daly in Hongkong will wish him every success in Shanghai.

On the 31st December, there were 262 steam launches employed in the harbour. Of these 105 were licensed for the conveyance of passengers. 134 were privately owned, 16 were

The House of Lords has read a second time the Bill enabling the High Court to compel a witness to be stated on points arising in criminal trials, and gives power to direct the retrial of cases tried at Quarter Sessions. The Bill, which now becomes law, is the outcome of the Beck case.

Ajax left Singapore yesterday p.m. and be expected here on or about the 22nd inst. The O. S. S. Co. & C. M. S. N. Co/s **Calcutta** left Shanghai 15th inst. for Hongkong via Poochow and is due here on the 20th. She will sail again for Europe on 21st inst.

TELEGRAMS.

[Reuter's.]

Fatal Explosion on H.M.S. "Magnificent."

LONDON, 15th June.
While H.M.S. "Magnificent" was at gun practice off Tatum, a six-inch shell exploded inside the gun. Eighteen, including the Gunner Lieutenant, were wounded.

The cartridge of the shell in the "Magnificent's" gun missed fire, whereupon the breech opened and an explosion occurred simultaneously.

Lieutenant Stobart and three men, injured by the explosion, are dead.

The Assassination of M. Delyannis.

The death of M. Delyannis is regarded in Greece as a national calamity and is causing serious confusion; funds generally are lower and the House was closed till after the funeral in order to avert a panic.

The Morocco Conference.

Mr. Lowther, British Minister at Morocco, presented a categorical refusal from Great Britain on the 8th June to join an international conference. Count Tattenbach on the 9th June conveyed Germany's acceptance.

Later.

Peace Preliminaries.

An official despatch from Washington states that Washington has been selected for a conference of the Plenipotentiaries.

France.

M. Rouvier has decided to retain for the present the Foreign portfolio.

Royal Marriage.

The marriage of the Crown Prince of Sweden and Princess Margaret of Connaught has taken place at St. George's Chapel, Windsor; the weather was magnificent.

THE COMING OF SANDOW.

In all probability Hongkong will shortly be having a visit from Sandow, the strong man, who is touring the East in the cause of physical culture. He was due at Penang on the 6th inst., and is booked to open at Singapore on the 16th inst. Mr. Sandow has with him a troupe of 20 pupils who assist him in his demonstrations.

These pupils are amateurs who have joined Mr. Sandow for a tour and the world to assist him in demonstrating his celebrated system of physical culture. In addition all appear nightly in his great Græco-Roman Arena. Mr. Sandow has just finished a phenomenally successful tour of India, appearing at the various theatres during his tour. It has been found that the theatres in the several parts of India are not spacious enough for holding the audiences that crush and jam around the doors nightly endeavouring to gain admission to see the World's Celebrated Strong Man; and to avoid this for the rest of his Eastern tour he has just had an enormous canvas theatre built. The performance presents that of an ancient Græco-Roman arena, the pupils during the performance assisting and playing their parts of ROMAN GLADIATORS, IN OLYMPIC GAMES, and all kinds of athletic sports. This company is probably the largest and most expensive that has ever visited the Orient and the performance given is one that appeals in every one. A contemporary, in speaking of Mr. Sandow's performances in Calcutta, said at the time that the reception accorded to Mr. Sandow was absolutely unparalleled in the records of Calcutta, and we opine that it must have more than satisfied Sandow himself of the absorbing interest which his visit to India aroused. It may truly be said that no show that has ever come to the East has proved such a big draw or roused an audience to such a pitch of enthusiasm as that of Mr. Sandow in Calcutta. Sandow says that he believes himself to be a far stronger man today than he was some seven years ago. He is now 36. He says that a man's strength will go on increasing with careful treatment until he is 45, which is, as a rule, the limit of human development. Sandow's measurements at the present day are as follows:—

Weight 14 stone 6 lbs., height 5 ft. 9.3 inches, neck 18 inches, chest 48 inches, chest expanded 63 inches, thigh 26 inches, knee 14 inches, calf 18 inches, ankle 8.2 inches, upper arm 19.1 inches, forearm 17 inches, wrist 7.2 inches.

SHIPPING accommodation at Antwerp is about to be extended at a cost of ten millions sterling, guaranteed by the Belgian Government, by the construction of a canal of five miles. The work will occupy ten years. When completed Antwerp will be the first port of the world.

A QUESTION THAT FAILED.

THE STORY OF A FAILURE TO REACH SASEBO.

The following article is written by the special correspondent of the *Kobe Herald*. Sasebo is present homelike sealed to all foreigners except the very few favoured ones summoned there on Government business, and although I set out for the famous Naval Station last week with the determination to get through, come what might, I had at last to abandon my mission as altogether beyond hope. By sitting tight at Haikū-junction, the nearest point which I was able to reach, I might perhaps have caught glimpses of the various important personages going to and fro, but the waiting would have been long and the results uncertain, so, as the recognised span of life is limited to three score years and ten, I hastened on to Nagasaki.

My intention had been to go to Sasebo and try to get information from the Captain of the *Oldhamia*, whose vessel was captured by the Russians near Formosa, and who had been transferred to a Russian vessel which came to grief during the battle. My information was that the *Oldhamia's* captain had been taken to Sasebo with the Russians, and this I found to be quite correct. Also, I hoped to have a look at the two famous men who have almost monopolised the attention of the world during the past few weeks, and even for journalists should not limit their ambitions to have a talk with them. But neither Togo nor Kojima was to come within my ken, nor was I to see Japan's huge, grim ships and note the difference between the present time and the period when they excited my admiration in the magnificent grandeur of the naval review two years ago.

I went with the roar of a lion, heralding my approach with telegrams and telephone messages to the lords-high-everybody likely to have influence. I returned like a lamb. And the entire staff of police at Haikū turned out to offer their sympathy, and possibly also to see that I did nothing which under martial law might be considered a sin.

So I went to Nagasaki. My ardours were considerably damped by this time, but neither the police nor my own Consul being able to help me, I reluctantly gave in and submitted to the inevitable. The man who goes to Sasebo first, I'll admit to be a better man than I am, but I can't help hoping that he will have to overcome all the opposition which I met myself.

THE RUSSIAN SAILORS AT NAGASAKI.

I found Nagasaki celebrating the nation's triumph on a scale which, considering its population, did it credit. Every now and again, there was the startling crack of some gigantic firework and a bright glare which took one's attention from the illuminated earth and water to the sky. But such things are of constant occurrence, and I did not go to Kishū to gaze at fireworks, lantern processions and similar commonplaces. I wanted something exclusive, something to keep the cables busy, and next morning I obtained at least some slight return for my trouble.

I discovered that fifty-two Russians were lodged in the two French hotels, so I went straight there, and on my way saw a few of them strolling around the streets. Big, healthy-looking fellows they were, too, with nothing about them to indicate that only a week before they had fought through one of the biggest naval battles in the history of the world. On arriving at the hotels, which are situated within an easy stone's throw of each other, I found several Russian seamen hovering around the door, near which some curio dealers were displaying their wares. They all looked in good health and condition, though they seemed rather subdued, and in no way suggested the rowdiness one had expected after the reports of their doings at the various ports at which they touched during the voyage. There were also present a couple of policemen, and a Japanese, who I learned was acting as interpreter. I may here remark that anyone who says that Kishū is under-policed can never have travelled outside the port of Nagasaki. There is some savage tribe which we are told cannot count beyond a hundred. I do not belong to that particular brand of savage myself, but my mathematical powers are also limited, and I cannot say how many police interviewed me in the course of my journeyings. Let me add, though, that I am rather pleased than otherwise at having become so well-known to the police, for I found them all excellent fellows. In the particular instance now referred to, the police outside the hotel refused to allow me to address any of the Russians, so I had to go to the police station and obtain the consent of the Superintendent.

THE LATE KORAN CHARGE.

CAPTAINS.

IN LONDON.

The late Korean Charge d'Affaires in London (writes a correspondent who knew him well) was a typical representative of the Hermit Kingdom. Of a naturally quiet and reserved—almost shy—disposition, he was known but little outside diplomatic and official circles, but to those who visited him at the Legation at Earl's Court he was at all times extremely courteous. He had acted as Charge d'Affaires since the departure of the Minister, though he had but a slight knowledge of the English language.

He was but rarely seen in Downing-street, and then only when some special business took him there; but he attended most of the levees, and other Court functions. Since the outbreak of war in the Far East his position has been a somewhat anomalous one, and he has been out of touch more or less with his Government.

The Consul-General for Korea in London is Mr. Pritchard Morgan, a former Liberal member for Merthyr Tydfil. Tragedies at the foreign embassies and legations in London are hardly of rare occurrence. In 1898 a determined attack was made upon Count Aro Valley, then First Secretary of the German Embassy in London. The diplomat was leaving the well-known corner mansion in Carlton House-terrace when he was twice fired at by a miscreant. One of the bullets entered his back whilst another struck an unfortunate policeman on point duty.

RUSSIA'S "SILVER LINING."

THE DAWN OF REFORM. PEASANTS AND THE LAND.

In the black sky of the Russian crisis there is a faint glimmer of the dawn. Imperial promises made in December last have an appearance of being kept. The Emperor has declared his willingness to grant the reforms, even if the people agitate. "But if they do, they will be punished," he is reported to have said. The Rescript on the subject of religions has made a favourable impression; some semblance of a Constitution seems to be shadowed forth in the deliberations of the State Council, and there are signs of a change of attitude towards the Press. The mountain of bureaucracy is in labour; it may produce a ridiculous mouse, but that is better than nothing. When it comes, this first dim outline of a Parliament—composed of two members from each class of the community: professional men, merchants, artisans, peasants, etc., appointed by the Zemstvos in the different provinces—will have the character of an advisory board offering advice to the autocrat. Those are, it seems the Imperial intentions as to a Constitution. More hopeful, too, is the outlook for the Press. When the "Birjewa-Viedomosti" offended the other day by publishing a feuilleton in which the Emperor was attacked under the disguise of a character in the story, the journal was not proceeded against administratively, but legally, by court of law. Usually, when a newspaper oversteps the narrow limits allowed it for discussion—so narrow because the carriages of the court had them—the police adopt one of a series of methods. Either they cut off the advertisements and thus deprive the organ of revenue, or they forbid the street sales; or perhaps, as an extreme measure, they suspend publication for a certain period. It is by no means uncommon to be told, when one inquires for a paper:—"Oh, that will appear in two months' time!"

In proceeding by legal means against the journal in question, the Government seems to have really initiated a new policy towards the Press.

In his famous Rescript of December, the Emperor laid it down that the Press should be dealt with under the ordinary laws. The language appears strange in the mouth of a European sovereign. Yet, to declare that the laws shall be followed is really to enunciate a new principle in Russia. Herein lies the trouble: the laws are not followed; they are a dead letter. In their place, reigns the police-made or Civil Service law. It has the significance of martial law; that is to say, of the absence of law and the substitution of arbitrary action. On paper, Russia has an excellent Constitution, with all the good qualities of paternal Government; but in practice it avails nothing except to accentuate the oppressive character of the actual administration.

OLD BELIEVERS AND JEWS.

Liberty of conscience has certainly received an ampler interpretation than might have been expected in the recent Rescript, granting to the Old Believers the practice of their faith. It is really a great step, and its beneficial character is acknowledged even by the Liberal Party in Russia. It is difficult to say exactly how many are affected, but I have heard it computed that they number twenty-five millions. Yesterday, a deputation of Old Believers tendered their homage to the Tsar at Tsarko-Selo, and expressed their gratitude for the privilege. These adherents of the Old Faith have known the bitterness of persecution. Two hundred years ago, the Patriarch Nikon, the supreme head of the Church, perceiving that there were grave errors in the version in use of the religious books, made extensive alterations in the text for the purpose of bringing it into line with religious practice. He imposed his authority (which is that of the Pope of Rome without his infallibility) upon the clergy and the great mass of the faithful, but a certain section refused to accept the new version, and continued with extreme conservatism their allegiance to the old. Burning and decapitation had not the effect of weakening the resistance of the Old Believers; on the contrary, the movement grew. It is in process of years' new prophet arose amongst the Old Believers, and the body was split into a multitude of sects, each holding fiercely to its own interpretation of the Word. Lately these dissidents have lived under a somewhat milder regime. Authority has shut its eyes to their nonconformity. The officials charged with their overlooking have accepted bribes not to pursue their investigations too far. Consequently the sectarians have lived more or less peacefully in a false atmosphere of orthodoxy, and practised their religion in secret, paying the Church dues, they have been unmolested. So that even before the last Rescript their position had become amenable. Yet the official indulgence now given to their belief is, of course, of value, and has resulted in the unsealing of ancient places of worship and the open practice of the religion by many who were nominally Orthodox for fear of the penalties attaching to opposition.

The lot of the Jews is not bettered by the Imperial ukase. Alexander II., who is supposed to have been less fervently religious than Alexander III. and Nicholas II., granted permission for a synagogue in St. Petersburg, but there is no extension of that privilege under the present regime. Jews are still shut out from positions of place and influence in the services and in the professions. A few may now practise as a barrister with permission of the Minister of Justice, but I am assured that for the past fifteen years there has been none of the faith admitted to the St. Petersburg bar, his position being limited to the subordinate one of licentiate, or of a student keeping his terms. Naturally, the removal of Jewish disabilities would constitute a reform even more striking than the recognition of the Old Believers. On the whole, one may say that the tendency of the moment is to treat Jews and all other opposing elements in the nation with greater leniency than for some time past. Liberalism attributes this new spirit to the chain-smoking influence of military reverses in the Far East and M. Witte's fear of the Terrorists. The Grand Duke Serge was a bitter enemy of Israel, permitting no Jews to live in Moscow. At St. Petersburg, however, and in other places, certain categories of Jewish residents are allowed—merchants and men following a trade.

THE PEASANT "ET MEUS REX."

One of the Easter gifts of the Tsar was the remission of debts weighing for years past upon the peasantry. The total of those debts is equivalent to a sum of seventy-five million roubles. But, seeing that the people could not possibly pay in any case, it is a gift more in

form than in reality. Moreover, the ultimate liquidator is the people themselves, though, nominally, the burden is to be borne by the nobles and rich landed proprietors. The great opposing force to the carrying out of reforms is the force of inertia of that great uneducated mass of peasantry which, impossible to interest in the subtleties of politics, knows only one sensation, the hunger for land. To the peasant there is God and the Tsar and the land—that is all. The Tsar is the appointed ruler of the flocks of the earth, the "Little Father" of the peasant. "There should be no intermediate classes," says and thinks the peasant; "ego et meus rex" is sufficient for the whole world of Russia. Each a disposition of mind makes the rude husbandman a most powerful instrument for the party of reaction.

The peasant is not patriotic in the European sense. He acknowledges as a painful necessity the existence of the State, present visibly in the person of the tax-gatherer, he supposes, in a blind way, that he must be protected from outside foes; but the land, always the land: that is the "ultima Thule" of his aspirations and political creed. A vast part of the land in Russia is peasant-held. The Mir, or village council, at stated intervals puts the common land to the ballot, and each inhabitant has his allotment. As he has no fixity of tenure, it is not to his interest to cultivate well, but to draw as much as possible from the soil. This system tends to land impoverishment, and its defects have frequently been pointed out; but the Government has always declined to make a change. It is necessary to know the limited horizon of the Russian peasant, his absolute detachment from all the great burning social topics of the time, to realise how impossible it is to enlist him under a banner of reform and revolution. But this does not mean that movements cannot be engineered of a greater or less magnitude.—MR. CHARLES DAWBARN.

COMMERCIAL.

WEEKLY SHARE REPORT.

In their report of the 16th inst., Messrs. Benjamin, Kelly and Potts state:—
During the past week, the market has remained dull and owing to the continued tightness of money and the further depreciation in our principal stocks, there has been very little business done.

Banks.—Hongkong and Shanghai Banks are slightly firmer, and have been placed to a small extent at \$300 and \$302½. The London quotation has risen to \$33 10/16. Nationals remain without change at \$37.

Marine Insurance.—Cantons have been disposed of at \$325, \$330 and \$325, closing with further sellers at the higher rate. China Traders have changed hands at \$64 and \$65, and at the latter rate more shares can be placed. Unions are required for at \$590, and Yangtzes have advanced to \$172½. There is no change in North Chinas.

Fire Insurance.—China Fires have found buyers at \$87, and Hongkong Fires, after sales at \$300 and \$302½, are offering at the latter price.

Shipping.—Further sales of Hongkong, Canton and Macao Steamboats have been effected at \$37. Indo-Chinas have not maintained their position, and have weakened to \$102, at which rate shares are obtainable. In reply to the request of the China shareholders for a bonus, the following telegram has been received by the general managers:—"It was decided at the meeting of 17th to declare a dividend at the rate of 10/- per share we (the Directors) are not in favour bonus, in view of keen opposition." China and Manly have declined to \$21, and are offering. Douglas Steamships have sellers at \$36, and Star Ferries are steady at quotations. Shell Transports are quoted in London at 27/6. Shanghai Tugs are in demand at 11s. 6d. and 11s. 5d. for the ordinary and preference shares respectively.

Refineries.—China Sugars have further weakened to \$215, at which rate shares have changed hands. Luxons are quieter after sales at \$33 and \$32 and more shares can be procured at the latter rate.

Mining.—Chinese Engineering have improved, and are in request at 7½. Raubs have been dealt in at \$4 and \$4½.

Docks, Wharfs and Godowns.—Hongkong and Whampoa Docks have fallen to \$107 at which price sellers prevail. Farnhams have been done at 11s. 16d. and 16½, and are now wanted at 11s. 16d. Referring to the directors proposal to reduce the capital of the company, we understand that a new company will be formed which will take over the undertaking of the old, and, as consideration, will pay to the registered holder of every share in the present company 11s. 20 in cash, and will allot to such registered shareholder a fully paid-up share in the new company of the nominal value of 11s. 80. The capital of the new company will be 11s. 4,416,000, divided into 55,200 shares of 11s. 80 each. The sum of 11s. 1,104,000 will be required for the cash payment to shareholders and arrangements have been made with the company's bankers for providing this amount subject to interest at 7½ per annum. Kowloon Wharfs have been parted with at the reduced rate of \$100 and further shares are offering. Hongkong Wharfs are easier and may be obtained at 11s. 16½. New Amoy Docks are to be had at \$18.

Lands, Hotels and Buildings.—Hongkong Lands are still in the market at \$122. Shanghai Lands have been bought at 11s. 120. Kowloon Lands are on offer at \$10 and West Points at \$55. Astor House Hotels have been sold at \$31. Hongkong Hotels continue weak at \$144, and Hotel des Colonies have been booked at 11s. 17½. Humphreys' Estate are quiet at \$13.

Cotton Mills.—Ewos have advanced and are required for at 11s. 36. Internationals and Laou-Kung-Mows are wanted at 11s. 36 and 11s. 39 respectively. Hongkong Cottons remain on offer at \$16½.

Cigar Factories.—Sumatras are procurable at 11s. 70.

Miscellaneous.—Green Island Cements are steady at \$16½ (old) and \$16½ (new). China Light and Powers have been done at \$10. There are sellers of China Borneos at \$12½. Electrics (old issue) have found buyers at \$17½; the new shares are offering after sales at \$11½. Business in A. R. Watson's has been transacted at \$12½. Hall and Holtz are reported sold at \$37. Langkats have been done at 11s. 230 ex the dividend of 11s. 5 paid in Shanghai yesterday.

YARN MARKET.

In their report, dated 16th instant, Messrs. Cawajee Pallaes and Co. write:—Our last week was dated 2nd instant per s.s. *Malta* since when a brisk demand has continued in this market and a good business has been put through; the bulk of sales reported being for superior spinnings, greatly checking their consumption in the interior. The market closes steady. No. 6s. have remained steady with moderate inquiry. No. 8s. continue neglected. No. 10s. the comparative cheapness of medium quality threads of this count has attracted by far the largest amount of attention and an increased business has been effected in this description to the neglect of superior but high priced spinnings. 10. 12s. a moderate business has been done in special chops of desirable spinnings at or about previous rates. No. 16s. in trifling request at unchanged prices. No. 20s. demand has unexpectedly subsided and only a small business has been reported. Sales during the past fortnight comprise of about 50 bales of No. 6s.; 6,550 bales of No. 10s.; 1,100 bales of No. 12s.; 400 bales of No. 16s.; and 975 bales of No. 20s. In all about 9,075 bales. Arrivals per steamer *Capt. and Namang* of about 11,220 bales. Shipment to Shanghai and Northern Ports about 7,000 bales. The unsold stock is estimated at about 27,000 bales.

Local Yarn.—An advance of 1½/- per bale has to be noticed in those threads, and sales of 150 bales of Nos. 10s. at \$10 and of 250 bales No. 12s. at \$11½, long delivery, has transpired. Japanese Yarn.—Sales reported are 225 bales No. 16s. at \$13½.

Exchange.—We quote to-day on India at Rs. 14½ per cent. London at 15. 10. 1/2.

TO-DAY'S EXCHANGE.

Selling.	
London—Bank T.T.	15/10 1/2
Do. demand	15/10 1/2
Do. 4 months' sight	15/10 1/2
France—Bank T.T.	237
America—Bank T.T.	141 1/2
Germany—Bank T.T.	193
India T.T.	141 1/2
Do. demand	141 1/2
Shanghai—Bank T.T.	71 1/2
Japan—Bank T.T.	52 1/2
Java—Bank T.T.	113 1/2
Buying.	
4 months' sight L/C.	15/10 1/2
6 months' sight L/C.	15/10 1/2
30 days' sight San Francisco & New York	46 1/2
4 months' sight do.	47 1/2
30 days' sight Sydney and Melbourne	1/11 3/16
4 months' sight do.	2/40 1/2
6 months' sight do.	2/42 1/2
4 months' sight Germany	1/7 1/2
Bar Silver	27 1/16
Bank of England rate	21 1/2
Sovereign	10/6 1/2

GRIUM QUOTATIONS.

To-day's quotations are as follow:—	
Malwa New	1,140
" Old	1,180
" Older	1,230 1/2, 260
" Oldest	1,340
Patna New	1,112 1/2
Benares New	1,080
Persian (Paper)	78/10 1/2

To-day's Advertisements.

PUBLIC AUCTION.

THE Undersigned have received instructions from the Official Administrator, to sell by PUBLIC AUCTION, For Account of the Estate of the late EDWARD L. HEATH,

on WEDNESDAY, the 21st June, 1905, at 11 A.M., at their Sales Rooms, No. 8, Des Voeux Road, corner of Ice House Street, THE GOODS AND CHATTELS of the above-named Deceased.

TERMS:—As usual. HUGHES & HOUGH, Government Auctioneers, Hongkong, 17th June, 1905. [664]

MADAME JAY'S, No. 4, DES VOEUX ROAD, Have just received, from New York, LADIES' AMERICAN SHOES, the new RUSSET-BROWN, (now so fashionable), Black Patent Leather and White Canvas with MILITARY HEEL. Also a fine lot of American STAYS and SHIRT WAISTERS. Hongkong, 17th June, 1905. [665]

BRITISH INDIA STEAM NAVIGATION COMPANY, LIMITED.

FROM RANGOON AND STRAITS. THE Company's Steamship

"ZAIDA," having arrived from the above Ports, Consignees of Cargo by her are hereby informed that their Goods will be delivered from alongside. Cargo impeding the discharge or remaining on board after 4 P.M. the 17th instant, will be landed at Consignees' risk and expense. No Fire Insurance will be effected. Bills of Lading will be countersigned by JARDINE, MATHESON & Co., Agents. Hongkong, 16th June, 1905. [662]

NOTICE TO CONSIGNEES. FROM CALCUTTA, PENANG AND SINGAPORE.

THE Steamship

"ARRATOON AFAR," having arrived from the above Ports, Consignees of Cargo are hereby informed that their Goods will be delivered from alongside. Cargo impeding the discharge will be landed at once, at Consignees' risk and expense.

Cargo remaining on board after the 20th instant, at 4 P.M., will be landed at Consignees' risk and expense into the Godowns of the Hongkong and Kowloon Wharf and Godown Co., Limited.

Consignees of Cargo from SINGAPORE and PENANG are requested to take IMMEDIATE DELIVERY of their Goods from alongside, such Cargo impeding the discharge of the vessel will be landed and stored at Consignees' risk and expense.

No Fire Insurance has been effected. Bills of Lading will be countersigned by the Undersigned.

DAVID SASSOON & Co., LIMITED, Agents. Hongkong, 17th June, 1905. [663]

Intimations.

ROBINSON PIANO COMPANY, LD.

THE PUBLIC MAY RELY IMPLICITLY ON GETTING FROM US PIANOS OF THE HIGHEST CLASS. EMBODYING THE VERY BEST MUSICAL AND WEARING QUALITIES, AT MODERATE PRICES.

OUR CLIENTS HAVE THE ADVANTAGE OF OUR 30 YEARS' EXPERIENCE AS EXPERTS, DEVOTED EXCLUSIVELY TO THE PIANO TRADE.

WE ARE BY FAR THE LARGEST IMPORTERS AND MANUFACTURERS IN CHINA, AND STOCK THE GREATEST VARIETY OF MAKES.

Hongkong, 8th June, 1905. [521]

CAFE WEISMANN.

THE Public are invited to pay a visit to our new TIFFIN ROOMS.

The only place of its kind in Hongkong.

A VERITABLE FAIRY LAND.

REAL GERMAN PASS BEER ON DRAUGHT.

Entrance—No. 14, WYNDHAM STREET.

Hongkong, 22nd April, 1905. [46]



THE POPULAR SCOTCH "BLACK & WHITE"



JAMES BUCHANAN & CO. SCOTCH WHISKY DISTILLERS. By Appointment to H.M. THE KING and H.R.H. THE PRINCE OF WALES

'Supplied' at all the LEADING CLUBS and HOTELS, and to be obtained from the principal Grocers. [178]

1997, 1998, 1999, 2000, 2001, 2002, 2003, 2004, 2005, 2006, 2007, 2008, 2009, 2010, 2011, 2012, 2013, 2014, 2015, 2016, 2017, 2018, 2019, 2020, 2021, 2022, 2023, 2024, 2025, 2026, 2027, 2028, 2029, 2030, 2031, 2032, 2033, 2034, 2035, 2036, 2037, 2038, 2039, 2040, 2041, 2042, 2043, 2044, 2045, 2046, 2047, 2048, 2049, 2050, 2051, 2052, 2053, 2054, 2055, 2056, 2057, 2058, 2059, 2060, 2061, 2062, 2063, 2064, 2065, 2066, 2067, 2068, 2069, 2070, 2071, 2072, 2073, 2074, 2075, 2076, 2077, 2078, 2079, 2080, 2081, 2082, 2083, 2084, 2085, 2086, 2087, 2088, 2089, 2090, 2091, 2092, 2093, 2094, 2095, 2096, 2097, 2098, 2099, 2100, 2101, 2102, 2103, 2104, 2105, 2106, 2107, 2108, 2109, 2110, 2111, 2112, 2113, 2114, 2115, 2116, 2117, 2118, 2119, 2120, 2121, 2122, 2123, 2124, 2125, 2126, 2127, 2128, 2129, 2130, 2131, 2132, 2133, 2134, 2135, 2136, 2137, 2138, 2139, 2140, 2141, 2142, 2143, 2144, 2145, 2146, 2147, 2148, 2149, 2150, 2151, 2152, 2153, 2154, 2155, 2156, 2157, 2158, 2159, 2160, 2161, 2162, 2163, 2164, 2165, 2166, 2167, 2168, 2169, 2170, 2171, 2172, 2173, 2174, 2175, 2176, 2177, 2178, 2179, 2180, 2181, 2182, 2183, 2184, 2185, 2186, 2187, 2188, 2189, 2190, 2191, 2192, 2193, 2194, 2195, 2196, 2197, 2198, 2199, 2200, 2201, 2202, 2203, 2204, 2205, 2206, 2207, 2208, 2209, 2210, 2211, 2212, 2213, 2214, 2215, 2216, 2217, 2218, 2219, 2220, 2221, 2222, 2223, 2224, 2225, 2226, 2227, 2228, 2229, 2230, 2231, 2232, 2233, 2234, 2235, 2236, 2237, 2238, 2239, 2240, 2241, 2242, 2243, 2244, 2245, 2246, 2247, 2248, 2249, 2250, 2251, 2252, 2253, 2254, 2255, 2256, 2257, 2258, 2259, 2260, 2261, 2262, 2263, 2264, 2265, 2266, 2267, 2268, 2269, 2270, 2271, 2272, 2273, 2274, 2275, 2276, 2277, 2278, 2279, 2280, 2281, 2282, 2283, 2284, 2285, 2286, 2287, 2288, 2289, 2290, 2291, 2292, 2293, 2294, 2295, 2296, 2297, 2298, 2299, 2300, 2301, 2302, 2303, 2304, 2305, 2306, 2307, 2308, 2309, 2310, 2311, 2312, 2313, 2314, 2315, 2316, 2317, 2318, 2319, 2320, 2321, 2322, 2323, 2324, 2325, 2326, 2327, 2328, 2329, 2330, 2331, 2332, 2333, 2334, 2335, 2336, 2337, 2338, 2339, 2340, 2341, 2342, 2343, 2344, 2345, 2346, 2347, 2348, 2349, 2350, 2351, 2352, 2353, 2354, 2355, 2356, 2357, 2358, 2359, 2360, 2361, 2362, 2363, 2364, 2365, 2366, 2367, 2368, 2369, 2370, 2371, 2372, 2373, 2374, 2375, 2376, 2377, 2378, 2379, 2380, 2381, 2382, 2383, 2384, 2385, 2386, 2387, 2388, 2389, 2390, 2391, 2392, 2393, 2394, 2395, 2396, 2397, 2398, 2399, 2400, 2401, 2402, 2403, 2404, 2405, 2406, 2407, 2408, 2409, 2410, 2411, 2412, 2413, 2414, 2415, 2416, 2417, 2418, 2419, 2420, 2421, 2422, 2423, 2424, 2425, 2426, 2427, 2428, 2429, 2430, 2431, 2432, 2433, 2434, 2435, 2436, 2437, 2438, 2439, 2440, 2441, 2442, 2443, 2444, 2445, 2446, 2447, 2448, 2449, 2450, 2451, 2452, 2453, 2454, 2455, 2456, 2457, 2458, 2459, 2460, 2461, 2462, 2463, 2464, 2465, 2466, 2467, 2468, 2469, 2470, 2471, 2472, 2473, 2474, 2475, 2476, 2477, 2478, 2479, 2480, 2481, 2482, 2483, 2484, 2485, 2486, 2487, 2488, 2489, 2490, 2491, 2492, 2493, 2494, 2495, 2496, 2497, 2498, 2499, 2500, 2501, 2502, 2503, 2504, 2505, 2506, 2507, 2508, 2509, 2510, 2511, 2512, 2513, 2514, 2515, 2516, 2517, 2518, 2519, 2520, 2521, 2522, 2523, 2524, 2525, 2526, 2527, 2528, 2529, 2530, 2531, 2532, 2533, 2534, 2535, 2536, 2537, 2538, 2539, 2540, 2541, 2542, 2543, 2544, 2545, 2546, 2547, 2548, 2549, 2550, 2551, 2552, 2553, 2554, 2555, 2556, 2557, 2558, 2559, 2560, 2561, 2562, 2563, 2564, 2565, 2566, 2567, 2568, 2569, 2570, 2571, 2572, 2573, 2574, 2575, 2576, 2577, 2578, 2579, 2580, 2581, 2582, 2583, 2584, 2585, 2586, 2587, 2588, 2589, 2590, 2591, 2592, 2593, 2594, 2595, 2596, 2597, 2598, 2599, 2600, 2601, 2602, 2603, 2604, 2605, 2606, 2607, 2608, 2609, 2610, 2611, 2612, 2613, 2614, 2615, 2616, 2617, 2618, 2619, 2620, 2621, 2622, 2623, 2624, 2625, 2626, 2627, 2628, 2629, 2630, 2631, 2632, 2633, 2634, 2635, 2636, 2637, 2638, 2639, 2640, 2641, 2642, 2643, 2644, 2645, 2646, 2647, 2648, 2649, 2650, 2651, 2652, 2653, 2654, 2655, 2656, 2657, 2658, 2659, 2660, 2661, 2662, 2663, 2664, 2665, 2666, 2667, 2668, 2669, 2670, 2671, 2672, 2673, 2674, 2675, 2676, 2677, 2678, 26

† Flagship of Rear-Admiral de Paigue de Josquieres, Second-in-Command.

Mails.

MESSAGERIES
MARITIMES

FRENCH MAIL STEAMERS.



STEAM FOR SAIGON,
SINGAPORE, BATAVIA,
COLOMBO, ADEN, EGYPT,
MARSEILLES, LONDON,
HAVRE, BORDEAUX,
MEDITERRANEAN AND BLACK SEA PORTS.

The S.S. "CALEDONNIEN."

Captain Gregory, will be despatched for MARSEILLES on TUESDAY, the 27th June, at 1 P.M.

Passage tickets and through Bills of Lading issued for above ports.

Cargo also booked for principal places in Europe.

Next sailings will be as follows:—

S.S. OCEANIC 11th July.

S.S. TOURANE 25th July.

S.S. TONKIN 8th August.

G. DE CHAMPEAUX, Agent.

Hongkong, 13th June, 1905.

THE PENINSULAR AND ORIENTAL
STEAM NAVIGATION COMPANY.

STEAM FOR
SAIGON, CEYLON, AUSTRALIA, INDIA,
ADEN, EGYPT, MEDITERRANEAN
PORTS, PLYMOUTH AND
LONDON.

(Through Bills of Lading issued for BATAVIA,
PERSIAN GULF, CONTINENTAL, AFRICA,
CAN and SOUTH AFRICAN PORTS.)

THE Steamship

"NUBIA,"
Captain F. J. Fox, carrying His Majesty's
Mails, will be despatched from this for
BOMBAY, on SATURDAY, the 1st July,
at Noon, taking Passengers and Cargo for
above Ports in connection with the Company's
S.S. Mongolia, 9,500 tons, from Colombo,
Passengers' accommodation in which vessel is
secured before departure from Hongkong.

Silk and Valuables, all Cargo for France
and Tea for London (under arrangement) will
be transhipped at Colombo into the Mail
steamer proceeding direct to Marseilles and
London; other Cargo for London, &c., will be
conveyed from Bombay by the R.M.S. *Arctia*,
due in London on the 13th August.

Parcels will be received at this Office until 4
P.M. the day before sailing. The Contents and
Value of all Packages are required.

For further Particulars, apply to

L. S. LEWIS,
Acting Superintendent.

Hongkong, 17th June, 1905.

NORTHERN PACIFIC LINE.

BOSTON STEAMSHIP COMPANY.

BOSTON TOW-BOAT COMPANY.

Connecting at Tacoma with
NORTHERN PACIFIC RAILWAY
COMPANY.

PROPOSED SAILINGS FROM HONGKONG FOR
VICTORIA, B.C., AND TACOMA,
VIA

MOJI, KOBE AND YOKOHAMA.

Steamer.	Tons.	Captain.	Sailing.
<i>Shawmut</i>	3,753	F.G. Purinton	At June 30
<i>Shawmut</i>	3,666	E. V. Roberts	July 12
<i>Tremont</i>	9,666	T.W. Garlick	Aug. 8

1 Cargo only.

CHEAP FARES, EXCELLENT ACCOMMODATION,
ATTENDANCE AND CUISINE, ELECTRIC
LIGHT, DOCTOR AND STEWARDESSES.

The twin-screw S.S. *Shawmut* and *Tremont*
are fitted with very superior accommodation
for first and second class passengers. The
large size of these vessels ensures steadiness
at sea. Electric fan in each room.

Barber's shop and steam-laundry. Cargo
carried in cold storage.

For further information, apply to
DODWELL & CO., LIMITED,
General Agents.

Queen's Buildings,
Hongkong, 23rd May, 1905.

ACHEE & CO.

ESTABLISHED 1859.

FURNITURE,

GENERAL HOUSEHOLD

REQUISITES.

&c., &c., &c.

Telephone 256.

AMATEUR WORK RECEIVES PROMPT AND CAREFUL ATTENTION.
Hongkong, 16th May, 1905.

For Sale.

GREEN ISLAND CEMENT COMPANY,
LIMITED.

PORTLAND CEMENT.

\$4.50 per Cask 375 lbs. net ex Factory.
\$2.70 per Bag 250 lbs. net ex Factory.

SHEWAN, TOMES & Co.,
General Managers.

Hongkong, 7th March, 1905.

TUBORG BEER.

A FIRST CLASS PILSENER BEER
guaranteed free from Salicylic Acid,
and any other Chemicals.

Price \$10.50 per case of 48 bottles (quarts)
or 6 doz. pints.

Special Prices for Quantities.

Sole Agents:—

SIEMSEN & CO.

Hongkong, 10th January, 1905.

FOR SALE.

INCANDESCENT
GASOLINE
LAMPS

OF ALL DESCRIPTIONS,
from the best makers.

INCANDESCENT

MANTLES,
CHIMNEYS,
GLOBES,
SHADES, &c.,

for

GASOLINE AND GAS

LAMPS

at the most moderate

prices.

Lamps fixed up for

Buyers free of charge.

Naphtha of the best

kind kept in stock.

TAI KWONG CO.,

56, Lyndhurst Terrace.

Hongkong, 2nd May, 1905.

To Let.

TO LET.

SHOP, No. 14, QUEEN'S ROAD, CEN-

TRAL.

First Floor, No. 12, QUEEN'S ROAD,

CENTRAL.

Second Floor, Nos. 12 and 14, QUEEN'S

ROAD, CENTRAL.

Apply to—

S. BISNEY,

Hongkong Hotel.

Hongkong, 8th June, 1905.

TO LET.

WITH IMMEDIATE POSSESSION.

"FOREST LODGE," Caine Road.

Apply to—

H. N. MODY.

Hongkong, 4th May, 1905.

TO LET.

NO. 12, KNUTSFORD TERRACE,

KOWLOON.

Apply to—

THE HONGKONG LAND INVEST-

MENT & AGENCY CO., LD.

Hongkong, 4th May, 1905.

TO LET.

A BUILDING AT CAUSEWAY BAY, at

present in occupation of the Steam

Laundry Co., Ltd.

No. 1, RIFON TERRACE.

FLATS IN MORETON TERRACE, facing

Polo Ground.

OFFICES in course of erection; CON-

NAUGHT ROAD (near BLAKE PIER).

GODOWNS: PRAYA EAST.

Apply to—

THE HONGKONG LAND INVEST-

MENT & AGENCY CO., LD.

Hongkong, 30th March, 1905.

TO LET.

SEMI-DETACHED VILLAS, Two, in

Garden Road, near the Ferry, with Fine

Bright and Airy Rooms. Gas and ELECTRIC

BELLS laid on. Commanding fine view of the

Harbour.

Rents very moderate.

Apply to—

H. RUTTONJEE,

No. 5, D'Aguiar Street,

37 and 38, Elgin Road, Kowloon.

Hongkong, 5th June, 1905.

SHARE QUOTATIONS.

Supplied by Messrs. BENJAMIN, KELLY & PORTS. Connected to noon; later alterations given under "Commercial Intelligence," page 5.

STOCKS.	NO. OF SHARES.	VALUE.	PAID UP.	POSITION AS PER LAST REPORT		LAST DIVIDEND.	APPROXIMATE RETURN AT PRESENT QUOTATION.	CLOSING QUOTATIONS.
				RESERVE.	AT WORKING ACCOUNT.			
BANKS.								
Hongkong & Shanghai Banking Corporation	20,000	\$125	\$125	{ \$1,000,000 \$8,000,000 \$350,000 }	\$1,493,408	{ Div. of £1.10/- and bonus of £1 @ ex- change 1/11 9/16=\$2.46 for second half-year 1904	12 1/2 %	{ \$80 1/2 sellers London 28 1/2 \$37 buyers
National Bank of China, Limited	99,925	£7	£5	\$200,000	\$41,768	\$2 (London 3/6) for 1903		
MARINE INSURANCES.								
Canton Insurance Office, Limited	10,000	\$250	\$50	{ \$1,400,000 \$1,739 }	\$150,494	\$17 for 1903	5 1/2 %	\$35 sellers
China Traders' Insurance Company, Limited	24,000	\$83.33	\$25	{ \$950,000 \$37,704 \$362,166 \$371,445 }	Nil.	\$4 1/2 for year ended 30.4.1904	7 1/2 %	\$65 buyers
North China Insurance Company, Limited	10,000	£15	£5	Tls. 800,000	Tls. 217,119	Interim of 7/6 1904	8 %	Tls. 82
Union Insurance Society of Canton, Limited	10,000	\$250	\$100	{ \$1,850,000 \$20,000 \$372,749 \$893,110 \$846,773 \$37,704 \$100,000 \$218,093 \$2,247 }	\$2,078,997	\$35 for 1903	5 %	\$60 buyers
Yangtze Insurance Association, Limited	8,000	\$100	\$60	\$700,000 \$37,704	\$486,284	\$12 and \$3 special dividend for 1903	8 1/2 %	\$172 1/2
FIRE INSURANCES.								
China Fire Insurance Company, Limited	20,000	\$100	\$10	{ \$185,000 \$100,000 \$218,093 \$2,247 }	\$329,047	\$6 dividend & \$1 bonus for 1903	8 1/2 %	\$87 sales
Hongkong Fire Insurance Company, Limited	8,000	\$250	\$50	\$1,200,505	\$360,372	\$34 for 1903	11 1/2 %	\$302 1/2 sellers
SHIPPING, TUG AND CARGO BOATS.								
China and Manila Steamship Company, Limited	30,000	\$25	\$25	{ \$5,000 \$185,439 }	\$8,831	\$1 for 1904	4 1/2 %	\$21 sellers
Douglas Steamship Company, Limited	10,000	\$50	\$50	{ \$250,000 \$500,000 \$158,444 }	Nil.	\$2 for year ended 30.6.1904	5 1/2 %	\$36 sellers
Hongkong, Canton & Macao Steamboat Co., Ltd.	80,000	\$15	\$15	{ \$205,000 \$100,000 }	\$26,160	\$1 for second half-year 1904	9 1/2 %	\$27
Indo-China Steam Navigation Company, Limited	60,000	£10	£10	{ \$200,000 \$100,000 }	£5,853	10/- for 1903 @ 1/10 5/16=\$5.378	4 1/2 %	\$102 sellers
Shanghai Tug and Lighter Company, Limited	200,000	Tls. 50	Tls. 50	{ Tls. 25,000 \$400,000 }	Tls. 43,764	{ Tls. 2 1/2 final making Tls. 4 1/2 for 1904 ... Tls. 1 1/2 final making Tls. 3 1/2 for 1904 ... Interim of 1/- (Coupon No. 5) for 1904 ... }	7 1/2 % 7 1/2 % 4 1/2 %	Tls. 60 buyers Tls. 50 buyers 22 1/2
"Shell" Transport and Trading Company, Limited	2,000,000	£1	£1	{ \$4,116 \$65,000 \$24,257 }	\$929	{ \$1.80 for year ending 30.4.1905 ... \$1.80 for year ending 30.4.1905 ... }	5 1/2 % 5 1/2 %	\$35 \$27
"Star" Ferry Company, Limited	10,000	\$10	\$5	{ \$24,257 \$400,000 \$21,075 \$130,153 }	\$21,231	\$10 for 1904	8 %	\$37 1/2 buyers
Straits Steamship Company, Limited	5,000	\$100	\$100	{ \$21,075 \$130,153 Tls. 120,000 Tls. 276,699 }	\$21,231	\$10 for 1904	8 %	\$37 1/2 buyers
Taku Tug and Lighter Company, Limited	30,000	Tls. 50	Tls. 50	{ Tls. 120,000 Tls. 276,699 }	Tls. 6,190	Final of Tls. 1 1/2 making Tls. 3 1/2 for 1904 ...	11 %	Tls. 30 sales
REFINERIES.								
China Sugar Refining Company, Limited	70,000	\$100	\$100	{ \$450,000 none }	\$42,812	Final of \$15 making \$20 for 1904	9 1/2 %	\$215 sales
Luzon Sugar Refining Company, Limited	7,000	\$100	\$100	{ \$450,000 none }	\$5,087	\$3 for 1897	38 %	\$22
Perak Sugar Cultivation Company, Limited	7,000	Tls. 50	Tls. 50	{ Tls. 100,000 none }	Tls. 1,635	Tls. 2 1/2 for year ending 30.9.04	38 %	Tls. 70
MINING.								
Chinese Engineering and Mining Company, Ltd.	1,000,000	£1	£1	{ £40,000 none }	£7,820	No. 3 of 1/6	5 1/2 %	Tls. 7 1/2 buyers
Oriental Consolidated Mining Company, Limited	50,000	G \$10	G \$10	{ none \$672,093 }	G \$672,093	50 cents making G. \$1 for 1904	5 1/2 %	G \$17 1/2
Raub Australian Gold Mining Company, Limited	50,000	£1	£1	{ £4,873 none }	£4,029	No. 12 of 1/-=48 cents		\$44 sales
SOCIÉTÉ FRANÇAISE DES CHARBONNAGES DU TONKIN.								
DOCKS, WHARVES & GODOWNS.								
Fairbank, (S. C.) Boyd & Co., Limited	55,000	Tls. 100	Tls. 100	{ Tls. 900,000 \$70,000 }	Tls. 48,153	Tls. 5 interim for 1904/5	8 %	Tls. 163 buyers
Geo. Fenwick & Co., Limited	6,000	\$25	\$25	{ \$18,423 \$10,000 \$300,000 \$250,000 \$33,500 }	\$8,577	\$3.75 for 1904	11 1/2 %	\$33 1/2
Hongkong & Kowloon Wharf and Godown, Co., Ltd.	40,000	\$50	\$50	{ \$100,000 \$300,000 \$250,000 \$33,500 }	\$29,422	Final of \$2 1/2 making \$5 for 1904	4 1/2 %	\$100 sellers
Hongkong and Whampoa Dock Company, Ltd.	50,000	\$50	\$50	{ \$50,000 \$33,500 }	\$498,289	{ \$6 dividend and \$1 bonus for 2nd half- year 1904	6 1/2 %	\$197 sellers
Hebrew Friskine, Limited	12,000	\$100	\$100	{ \$55,500 none }	\$489	\$10 div. & \$5 bonus for year end. 30.6.04	4 1/2 %	\$170 buyers
New Army Dock Company, Limited	6,000	\$64	\$64	{ \$150,000 none }	\$40,936	\$1 1/2 for 1903	7 1/2 %	\$18 sellers
Riley Hargreaves & Co., Limited	7,500	\$100	\$100	{ \$150,000 none }	\$40,936	\$10 div. and \$2 1/2 bonus for 1903	6 1/2 %	\$245 sellers
Shanghai and Hongkong Wharf Company	37,000	Tls. 100	Tls. 100	{ Tls. 48,210 Tls. 59,880 \$206,045 Tls. 17,500 }	Tls. 10,711	\$7 dividend	6 1/2 %	\$11 1/2
Tanjong Pagar Dock Company, Limited	37,000	\$100	\$100	{ Tls. 48,210 Tls. 59,880 \$206,045 Tls. 17,500 }	Tls. 10,711	Final of Tls. 6 making Tls. 10 for 1904	6 1/2 %	Tls. 18 1/2 sellers
Yangtze Wharf and Godown Company, Limited	2,500	Tls. 100	Tls. 100	{ Tls. 48,210 Tls. 59,880 \$206,045 Tls. 17,500 }	Tls. 2,762	\$20 for 2nd half year making \$26 for 1904	9 1/2 %	\$395
LANDS, HOTELS & BUILDINGS.								
Astor House Hotel Company, Limited (Shanghai)	30,000	\$25	\$25	{ none Tls. 34,000 Tls. 8,000 }	\$9,989	\$2 1/2 for year ended 30.6.1904	7 1/2 %	\$31
Astor House Hotel, Limited (Tientsin)	2,000	Tls. 150	Tls. 150	{ Tls. 3,000 Tls. 8,000 }	Tls. 806	Final of Tls. 5 making Tls. 9	8 1/2 %	Tls. 140 sellers
Central Stores, Limited	6,000	\$15	\$15	{ Tls. 6,000 \$20,000 }	\$1,502	Final of 60 cents making \$1.80 for 1904 ...	6 1/2 %	\$21 sellers
Do. (Founders)	123	\$15	\$12	{ \$20,000 none }		None	6 1/2 %	\$100
Do. (New Issue)	24,000	\$15	\$7 1/2	{ \$20,000 none }		Preferential of 7 per cent for 1904	6 1/2 %	\$8 sellers
Hongkong Hotel Company, Limited	12,000	\$50	\$50	{ \$100,000 \$10,000 }	\$3,554	\$5 for second half-year making \$10 for 1904	7 %	\$144 sellers
Hongkong Land Investment and Agency Co., Ltd.	50,000	\$100	\$100	{ \$250,000 \$37,875 }	\$37,875	Final of \$6 making \$12 for 1904	10 1/2 %	\$122 sellers
Hotel des Colonies Company, Limited (Shanghai)	9,000	Tls. 25	Tls. 25	{ Tls. 20,000 Tls. 20,000 }	Tls. 7,202	Tls. 2 1/2 for the year ending 31.3.1905	11 %	Tls. 17 1/2 sales
Humphreys Estate & Finance Company, Limited	150,000	\$10	\$10	{ \$200,000 \$50,000 }	\$11,958	90 cents for 1904	7 1/2 %	\$15 sellers
Kowloon Land and Building Company, Limited	6,000	\$50	\$30	{ none \$377 }	\$377	\$5 for 1904	7 1/2 %	\$40 sellers
Shanghai Land Investment Company, Limited	52,000	Tls. 50	Tls. 50	{ Tls. 828,813 Tls. 170,000 }	Tls. 40,066	{ Tls. 3 final and Tls. 2 bonus making } Tls. 8 for 1904	6 1/2 %	Tls. 120 buyers
Tientsin Hotel des Colonies, Limited	1,400	Tls. 50	Tls. 50	{ none Tls. 170,000 }	Tls. 670	Final of Tls. 5 for 1904	10 1/2 %	Tls. 47 sellers
Tientsin Land Investment Company, Limited	7,726	Tls. 100	Tls. 100	{ Tls. 67,300 none }	Tls. 725	Final of Tls. 4 making Tls. 7 for 1904	5 1/2 %	Tls. 125 sellers
Wei-hai-wei Land and Building Company, Limited	3,764	Tls. 25	Tls. 25	{ none none }	Tls. 5,150	None	5 1/2 %	Tls. 12 buyers
West Point Building Company, Limited	12,500	\$50	\$50	{ none none }	\$1,247	Final of \$1.70 making \$3.20 for 1904	5 1/2 %	\$55 sellers
COTTON MILLS.								
Ewo Cotton Spinning and Weaving Company, Ltd.	15,000	Tls. 50	Tls. 50	{ none none }	Tls. 11,651	Tls. 4 for year ended 31.10.1903	11 1/2 %	Tls. 38 buyers
Hongkong Cotton Spinning, Weaving and Dyeing Company, Limited	125,000	\$10	\$10	{ none none }	\$22,862	50 cents for the year ending 31.7.04	3 %	\$16 1/2 sellers
International Cotton Manufacturing Company, Ltd.	10,000	Tls. 75	Tls. 75	{ Tls. 50,000 Tls. 35,227 }	Tls. 13,629	Interim of 3 % a/c 1898		Tls. 36 buyers
Laou-kung-mow Cotton Spinning & Weaving Co., Ltd.	8,000	Tls. 100	Tls. 100	{ Tls. 35,227 none }	Tls. 10,000	Interim of 4 % a/c 1898 on 6,000 shares ...		Tls. 39 buyers
Soy Chee Cotton Spinning Company, Limited	2,000	Tls. 500	Tls. 500	{ Tls. 8,115 none }	Tls. 22,050	4 % for 1897		Tls. 180
CIGARS AND TOBACCO COS.								
Alhambra, Limited	300	\$200	\$200	{ none none }	Dr. P. 2,584	\$125 for year ending 30.6.1900		\$100
Philippine Company, Limited	7,500	\$10	\$10	{ Tls. 24,820 Tls. 25,000 }	Tls. 1,091	First year		\$9 1/2 sellers
Shanghai-Sumatra Tobacco Company, Limited	30,000	Tls. 20	Tls. 20	{ Tls. 24,820 Tls. 25,000 }	Tls. 1,091	Final of Tls. 6 making Tls. 9	13 %	Tls. 70 sellers
MISCELLANEOUS.								
Anglo-German Brewing Company, Limited	4,000	\$100	\$100	{ none none }	£161	First year		\$125 sellers
Bells Asbestos Eastern Agency, Limited	8,604	12/6	12/6	{ none none }	\$1,828	6d. per share for 1903	5 1/2 %	\$13 buyers
Campbell, Moore & Co., Limited	1,200	\$10	\$10	{ none \$8,000 }	Nil.	\$3 for 1904	8 1/2 %	\$36
China-Borneo Company, Limited	60,000	\$12	\$12	{ none Tls. 30,000 }	Tls. 718	\$1 for 1904	8 1/2 %	\$124 sellers
China Flour Mill Co., Limited	4,000	Tls. 50	Tls. 50	{ Tls. 30,000 none }	\$3,739	Tls. 5 for 1904	8 %	Tls. 304 sales
China Light and Power Company, Limited	30,000	\$10	\$10	{ none \$8,000 }	\$1,581	None	9 1/2 %	\$28
China Provident Loan & Mortgage Company, Ltd.	100,000	\$10	\$10	{ none \$8,000 }		80 cents for 1904	9 1/2 %	\$28
Dairy Farm Company, Limited	25,000	\$7 1/2	\$6	{ \$12,500 \$2,706 }	\$2,706	\$1 1/2 for year ending 31.7.1903	7 1/2 %	\$17 sellers
Fraser and Neave, Limited	4,500	\$50	\$50	{ \$12,500 \$2,706 }	\$2,706	\$5 div. and \$2 1/2 bonus for 1903	7 1/2 %	\$100 buyers
Green Island Cement Company, Limited	100,000	\$10	\$10	{ \$25,000 \$25,000 }	\$95,054	\$2 for 1904	7 1/2 %	\$64
Do. (New Issue)	50,000	\$10	\$5	{ \$25,000 \$188,000 }	\$7,551	First year	9 1/2 %	\$17 sales
Hall & Holtz, Limited	2,000	\$20	\$20	{ £23,109 £3,000 }	£7,625	Final of \$14 making \$21	9 1/2 %	\$17 sales
Hongkong & China Gas Company, Limited	7,000	£10	£10	{ £23,109 £3,000 }	£7,625	£1 div. and 2/- bonus for 1903	7 1/2 %	\$160 buyers
Hongkong Electric Company, Limited	30,000	\$10	\$10	{ none none }	\$1,747	{ \$1.00 30 cents } for year ending 30.4.1904	5 1/2 %	\$17 1/2 sellers
Hongkong High-Level Tramways Company, Ltd.	30,000	\$10	\$5	{ none \$5,000 }	\$2,796	\$15 for year ending 30.11.1904	7 1/2 %	\$113 sellers
Hongkong Ice Company, Limited	1,250	\$100	\$100	{ \$50,000 \$50,000 }	\$5,356	Final of \$13 making \$17 for 1904	7 %	\$242 1/2
Hongkong Rope Manufacturing Company, Ltd.	5,000	\$25	\$25	{ \$50,000 \$50,000 }	\$11,137	\$10 for 1904	7 %	\$152 buyers
Hongkong Steam Waterboat Company, Limited	15,000	\$10	\$10	{ \$2,500 \$2,500 }	\$299	Final of 70 cts. and 50 cts. bonus making \$1.20 for the year ending 30.9.04	11 1/2 %	\$17
Katz Brothers, Limited	10,000	\$100	\$100	{ \$475,000 \$475,000 }	\$3,400	\$8 for 1904	6 %	\$135 buyers
Lane, Crawford & Co., Limited (Shanghai)	2,500	\$100	\$100	{ Tls. 528,210 Tls. 19,465 }	\$21,582	Interim of \$5	7 1/2 %	\$145 sales
Maatschappij tot Mijn. Bosch-en Landbouw- plaat in Langkat, Limited	25,000	Gs. 100	Gs. 100	{ Tls. 528,210 Tls. 19,465 }	Tls. 35,849	{ and quarterly of Tls. 5, paid 15.6.05 mak- ing so far Tls. 12 1/2 for 1905 ... \$2 for year ended 31.10.1904	15 1/2 %	Tls. 220 exd. s.
Maynard and Company, Limited	3,400	\$10	\$10	{ none none }	Dr. Tls. 117,638	Tls. 5 for 1902	9 %	Tls. 25 sales
Mondon, (E. L.) Limited	7,000	Tls. 50	Tls. 50	{ none none }	\$832	Final of \$3 making \$5 for the year ending 30.6.04	9 %	\$54 sales
Moutrie & Company, Limited	4,000	\$50	\$50	{ \$8,000 none }		None		\$50
Shanghai & Hongkong Dyeing and Cleaning Co., Ltd.	1,200	\$50	\$50	{ None Tls. 145,000 }	Dr. \$5,537	Final of Tls. 5 making Tls. 1 1/2 for 1904 ...	7 %	Tls. 225 sales
Shanghai Gas Company, Limited	16,000	Tls. 50	Tls. 50	{ Tls. 108,172 Tls. 45,000 }	Tls. 10,247	Tls. 5 for 1903	6 %	Tls. 82 1/2 sales
Shanghai Horse Bazaar Company, Limited	5,400	Tls. 50	Tls. 50	{ Tls. 45,000 Tls. 25,000 }	Tls. 6,968	Final of Tls. 8 making Tls. 14 for 1904	8 1/2 %	Tls. 170 sales
Shanghai Pulp and Paper Company, Limited	4,500	Tls. 100	Tls. 100	{ Tls. 25,000 Tls. 17,220 }	Tls. 7,220	Final of 37 1/2 making \$26 for 1904	4 1/2 %	Tls. 450 buyers
Singapore Dispensary, Limited	6,000	\$50	\$50	{ \$20,000 none }	Dr. \$39,020	\$2 for year ended 31.7.1904	7 1/2 %	\$80
South China Morning Post, Limited	6,000	\$25	\$25	{ none none }		None	7 1/2 %	\$25
Team Laundry Company, Limited	5,000	\$5	\$5	{ none none }	\$3,544	60 cents for year ended 31.5.04	7 1/2 %	\$8 buyers
Straits Ice Company, Limited	10,000	\$5	\$5	{ none none }	\$700	First year	13 1/2 %	\$74 buyers
Straits Trading Company, Limited	250,000	\$10	\$10	{ \$25,000 \$25,000 }	\$84,813	\$1 div. and 35 cents bonus for half year ended 30.6.1904	6 1/2 %	\$44 sellers
Tientsin Native City Waterworks Company, Ltd.	2,941	Tls. 100	Tls. 100	{ none Tls. 15,259 }	Tls. 2,221	Tls. 2 for half year	6 1/2 %	Tls. 100
Tientsin Waterworks Company, Limited	2,000	Tls. 100	Tls. 100	{ Tls. 15,259 Tls. 2,221 }	Tls. 2,221	Final of Tls. 4 making Tls. 8 for 1903/4 ...	6 1/2 %	Tls. 125
United Asbestos Oriental Agency, Limited	9,000	\$10	\$10	{ \$20,000 \$20,000 }	\$480	90 cents for year ending 31.5.1904	9 1/2 %	\$100 buyers
Do. (Founders)	100	\$10	\$10	{ \$20,000 \$20,000 }		\$29.70 for year ended 31.5.1904	10 1/2 %	\$180
Watson, (A. S.) & Co., Limited	50,000	\$10	\$10	{ \$300,000 \$25,000 }	\$6,090	Final of 50 cents making \$1 for 1904	8 %	\$124 sales
William Powell, Limited	12,000	\$10	\$10	{ \$300,000 \$25,000 }	\$688	Interim of 50 cents for year 1904/1905	10 1/2 %	\$114 sellers